217,000 SF ON 25.49 ACRES AVAILABLE FOR LEASE

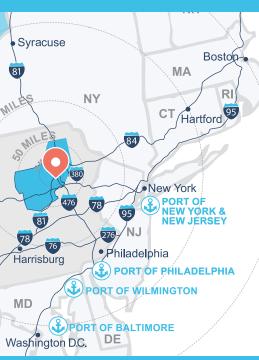
130-150 INDEPENDENCE DRIVE (P30)

CENTERPOINT COMMERCE & TRADE PARK PARK - EAST PH IIB PITTSTON TOWNSHIP, PA 18640



TAX-ABATED BUILDING NEAR I-81, I-476

LERTA APPROVED! 10-YEAR, 100% REAL ESTATE TAX ABATEMENT ON IMPROVEMENTS!



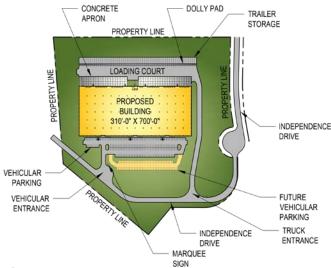
CENTERPOINT FEATURES

- Park is less than one mile from I-81 and I-476.
- ✓ Multi-year, 100% real estate tax abatement on improvements (LERTA).
- Immediate proximity to FedEx Ground, FedEx Express, FedEx Smart Post, and UPS.
- Park is home to industrial facilities for Lowe's, The Home Depot, Neiman Marcus, Bimbo Bakeries, Isuzu, Corning, and others.
- More than 700,000 people live within 30 miles.
- Two hours from New York City and Philadelphia.
- More than 51 million people live within 200 miles.
- Public water, sewer, natural gas, fiber, and power.
- Centrally located within Scranton/Wilkes-Barre labor market.





SITE PLAN



SIZE

- Available Space: 217,000 SF
- Acreage: 25.49 acres
- **Building Dimensions**: 310' (width) x 700' (length)
- Tenant space availability ranges from 30,687 SF to 217,000 SF.

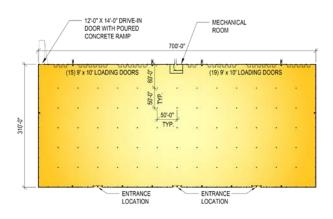
BUILDING CONSTRUCTION

- **Floor**: 7" thick concrete floor slab, reinforced with welded steel mats. Floor is treated with *SpecHard* siliconate sealer/densifier and *E-Cure* curing compound.
- **Roof**: Butler Manufacturing, MR-24 metal roof system.
- Exterior Walls: Architectural masonry, aluminum/ glazing entrance systems and metal wall panels with insulation.
- **Clear Ceiling Height**: 38'-2" approximate clear structural height along loading dock wall.
- **Bay Spacing**: 50'-0" x 50'-0" bay spacing with a 60'-0" deep staging bay.
- The building contains 3'-0" high x 6'-0" wide clerestory windows.

LOADING

- Loading: Single-sided loading.
- **Dock Equipment**: Thirty-four (34) 9'-0" x 10'-0" vertical lift dock doors with vision kits by *Haas Door* or equal with 40,000 lb capacity mechanical levelers with bumpers by *Rite-Hite* or equal.
- One (1) 12'-0" x 14'-0" vertical lift drive-in door by Haas Door or equal and a reinforced concrete ramp.

FLOOR PLAN



UTILITIES

- HVAC: Energy-efficient, roof-mounted Cambridge direct-fire units.
- **Electrical Service**: 800 Amp, 480/277 Volt, 3-phase service (expandable to 4,000 Amps).
- **Lighting**: Energy-efficient LED fixtures.
- **Fire Protection**: Early Suppression Fast Response (ESFR) wet sprinkler system.
- Utilities: All utilities shall be separately metered.
- Provisions for domestic water and natural gas shall be provided.

PARKING

- Asphalt paving, including heavy duty pave at truck areas and light duty pave in vehicle parking areas.
- On-site parking for approx. 191 vehicles with future parking for up to 72 additional spaces.
- On-site trailer storage for approx. 61 trailers with 8' wide concrete dolly pad.
- Asphalt paving, including heavy duty pave at truck areas and light duty pave in vehicle parking areas.

LOCATION

- Foreign Trade Zone status possible.
- Centrally located within Scranton/Wilkes-Barre labor market.

Northeastern Pennsylvania is the center of the Boston/ Washington Corridor and is at the confluence of five major highways. Interstates 81, 80, 84, 380 and 476 meet at this inland hub and provide excellent access to firms wishing to serve Maine to Virginia and west to Ohio.

Syracuse

NY

78

Philadelphia

PORT OF WILMINGTON

PORT OF BALTIMORE

380

476

78

MD

VA

Harrisburg

Washington D.C.

300 MILES

Buffalo

PENNSYLVANIA

Toronto

Erie_

Pittsburgh

WV

CENTRALLY LOCATED ON NORTHEASTERN PENNSYLVANIA'S I-81 CORRIDOR

DEEP WATER PORTS

PORT	MI	KM
Philadelphia	120	193
New York/New Jersey	121	195
Wilmington	132	212
Baltimore	191	307

TRAVEL DISTANCES

CITY	MI	KM
Delaware Water Gap, PA	57	92
Allentown, PA	67	108
Morristown, NJ	96	155
Philadelphia, PA	113	182
Harrisburg, PA	116	187
Port Newark, NJ	126	203
New York, NY	128	206
Syracuse, NY	152	245
Baltimore, MD	194	312
Hartford, CT	198	319
Washington, DC	237	381
Pittsburgh, PA	290	467
Boston, MA	301	484

10-YEAR, 100% REAL ESTATE TAX ABATEMENT ON IMPROVEMENTS

NH¹

MA

New York
PORT OF

ORT OF PHILADELPHIA

CT Hartford 95

NEW YØRK & NEW JERSEY

Boston

RI

EMPLOYMENT & UNEMPLOYMENT DATA

FOR LACKAWANNA & LUZERNE COUNTIES MORE THAN 470,000 PEOPLE LIVE WITHIN 20 MILES OF CENTERPOINT

YEAR	NEPA REGION CIVILIAN LABOR FORCE	EMPLOYED	UNEMPLOYED	NEPA	PA	US
2020	265,000	242,800	22,100	8.4%	7.3%	6.9%
2019	263,600	251,500	12,100	4.6%	3.9%	3.8%
2018	264,800	250,300	14,600	5.4%	4.8%	4.1%



LOCAL ECONOMIC REVITALIZATION TAX ASSISTANCE PROGRAM (LERTA)

Companies that move into CenterPoint have the opportunity to dramatically lower their operating costs due to a long established tax abatement program. Every building constructed in Mericle's CenterPoint Commerce & Trade Park receives a 10-year, 100% real estate tax abatement on improvements via the Local Economic Revitalization Tax Assistance (LERTA) program. This abatement program was approved by Luzerne County, the Pittston Area School district, Jenkins Township, and Pittston Township.

The annual real estate taxes you will save when you move into a LERTA-approved building in CenterPoint may vary depending upon the cost of construction required to meet your exact specifications.

Contact Bob Besecker, Jim Hilsher, or Bill Jones of Mericle's Leasing Team for a detailed savings estimate.



DEVELOPMENT DIVISION

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To learn more, please call one of us at **570.823.1100** to request a proposal and/or arrange a tour.



CONCEPTUAL SITE PLAN

PROPOSED 217,000 S.F. BUILDING



INDEPENDENCE DRIVE

FUTURE

PARKING

TRUCK

ENTRANCE

VEHICULAR



VEHICULAR PARKING 191 **FUTURE PARKING** TRAILER STORAGE **LOADING DOORS DRIVE-IN DOOR**

72 61 34 1 ACREAGE 25.49

> These plans have been prepared solely for marketing purposes for the exclusive use of -- Mericle Commercial Real Estate Services

570.823.1100 mericle.com

CONCRETE

PROPERTY LINE

LOADING COURT

PROPOSED BUILDING.

APRON

PROPERTY LINE

VEHICULAR PARKING

VEHICULAR

ENTRANCE

DOLLY PAD

PROPERTY LINE

INDEPENDENCE

DRIVE

MARQUEE

SIGN

TRAILER

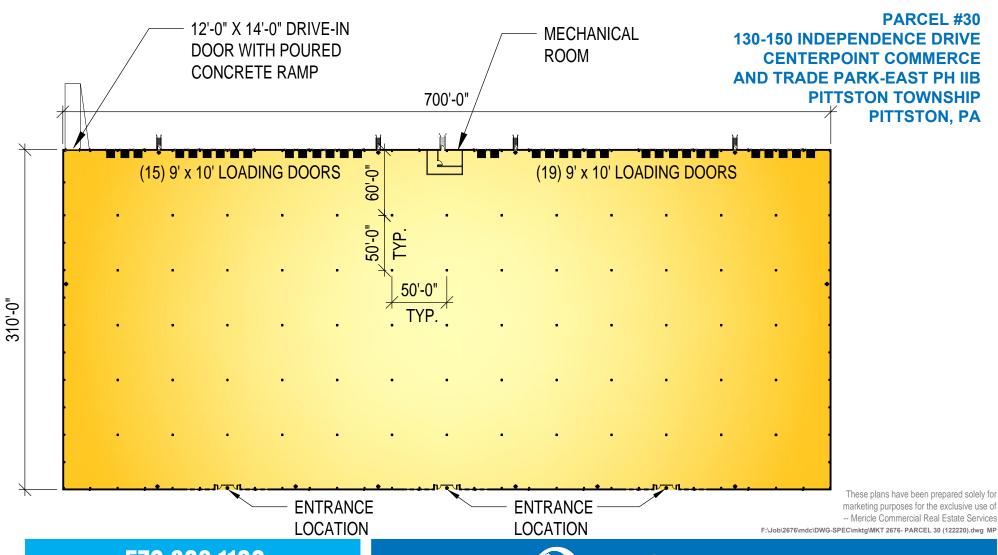
STORAGE



CONCEPTUAL FLOOR PLAN

PROPOSED 217,000 S.F. BUILDING





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